



THE PIEDMONT DIVISION

SOUTHEAST REGION, NATIONAL MODEL RAILROAD ASSOCIATION

TIMETABLE

VOLUME 27, No. 3

Effective 12:01 am, 12 July 2023



PIEDMONT DIVISION

SOUTHEASTERN REGION
NATIONAL MODEL RAILROAD ASSOCIATION



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VOLUME 27

No. 3

Effective 12:01am, Eastern Time
TUESDAY, JULY 11, 2023

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Deadline for the
4th Quarter 2023 TIMETABLE:
Wednesday, August 9, 2023

Send Submissions to:
timetable@piedmont-div.org

"WE'RE ALL ABOUT THE TRAINS"



START 'EM YOUNG

I have featured basement consuming model railroads in these pages, so why not a family room consuming Brio train layout on the cover?

Easton Jankauskas' Brio train collection could by itself be a featured layout in the *TIMETABLE*. Dad Justin's layout is equally impressive and—even though there is a duck-under—you don't have to get on the floor to play with it.

Easton clearly LOVES trains of all sizes and types. I am sure he will be a valued NMRA member at some point.

Working at train shows has shown that there are many youngsters out there like Easton. I have setup the Ambassador layout to give these kids a memorable experience.

For toddlers, I have them simply sound the horn and bells. Their expressions of amazement is often followed by an attempt to press all the other buttons just to see what else will happen.

I let preschoolers also run the active locomotives to slow them to switch tracks and speed up the hill. I am surprised that only half try to attain warp speeds, the others actually show surprising restraint.

I have a switcher in an engine house for the primary schoolers to run. They pull some passenger cars at a station and then have to spot the cars and run the engine around them to hook to the front before running around the layout. After a few loops they have to reverse the process to return everything back where they were.

For older kids, I have them pull the passenger cars and then switch them with cars at a second station. I let them have complete control of the throttle while I switch turnouts but only at their direction. I have only had a couple of kids do this so far, but each time they have come back to do it again because they think they can do it faster.

Of particular note is the parents' reactions. They are amazed at how interested their kids are in running the trains and are surprised about how much control and interaction today's model trains offer. I will praise the children's problem solving ability when they figure out which switches will get their engine on the right track. This has often resulted in a discussion about all the aspects of the hobby and how there is something for everyone in their family.

But is the initial interaction with the kids that gets us there.

—Jim Datka
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PIEDMONT DIVISION SUPERINTENDENT'S REPORT
 SPRING 2023



Greetings and welcome to Summer! Summer, like Winter, is a great time for Model Railroading! It is way too hot outside, the basement, nice and cool, is calling. There are projects just waiting on completion and, unless you love to do things like kayak, being cool, in the Summer, is awesome! Yeah, I know, I must cut the grass and mulch the planting beds. Sigh...

In any event, the Summer, here in our Division, is shaping up to be fun and a bit more relaxing for us, as we prepare to host the Southeastern Region Convention, in September! From there, we'll head into our very busy Fall with our annual Piedmont Pilgrimage! With those highlights out of the way, on with the report.

Of special note, the Board of Directors has approved our Sponsored Area Groups (SAG) concept. While the entire Guidelines document is included in this issue of the *Timetable*, I wanted to make note of it here. The SAGs will be groups that bring the Piedmont Division TO people in the farther reaches of our Division. We all know that Atlanta traffic is horrible so being able to attend Division events, but much closer to home, should be of great benefit to our members. These groups, while forming on their own, will be able to receive financial support from the Division to hold events. These events can range from monthly meeting watch parties to local Train 'N Camps on any subject of interest to the group's members, but all conducted away from Atlanta itself. We've already heard of interest in forming groups down South, up the I-85 corridor, up I-75, and potentially way out East. The August meeting will feature more information about the SAGs and their events and a contest. The Division Website will include even more information as it is developed. Please stay tuned!

Our website continues to develop. We are revising our process for adding events to the website. The process is being simplified and a couple more changes are being made so it's easier to identify events that are for members only. This includes things like our Train 'N Camps and our monthly, member-only, open houses. It is nice that, with the new hosting software, we can simply make changes without having to redo a ton of coding, or worse, not being able to make these changes at all. As the Region works on its own website rebuild, hopefully that team will incorporate similar features.

In July, we will resume an activity that has been very popular in the past. Our monthly meeting will not feature a clinic, but will instead feature a pizza party and swap meet. Members can bring a small number of items, of any type or variety, to sell or trade, during the meeting. The Division will be supplying the pizza and beverages, all the members need do is come and enjoy. One of the things that makes this event so popular, besides the great deals, is the chance to just enjoy the fellowship with members of our great hobby. Each time we've done this in the past the time is filled with great conversation, lots of stories, and a great deal of bargaining. I've bought some very cool things for my railroad at previous pizza nights. I'm looking forward to our next one and seeing what people have brought. Because there will be no clinic, this meeting will not be presented via Webex.

(continued on next page)

DIVISION SUPERINTENDENT'S REPORT—SPRING 2023

(CONTINUED)

In August, we will return to our clinic format. Jim Davis, a really cool guy and a real railroad employee, will be talking about using a Cricut Machine for modeling. This sounds interesting to me as I'm interested in what it can do to help with my own model railroad. As I understand it, a Cricut Machine can be used to cut things like styrene plastic to form walls and such. This would be awesome for me as I'm planning an area on my own railroad that will feature mostly older brick structures with some track snaking through them. Being able to design and construct large brick structures that could then have the outsides suitably detailed, would be a huge time and cost saver. I am intrigued.

The big event in September will, of course, be the *CARTERSVILLE EXPRESS* Southeastern Region Convention in Cartersville at the Clarence Brown Convention Center. If you have not seen this place at one of our Model Train Shows, you are in for a treat. The place is amazing and the staff there is absolutely a joy to work with. In any event, it really sounds like this is going to



be a great convention. Walt Liles and his team are working away to produce a convention that should be awesome. Just the banquet alone sounds like fun, but with Ops sessions, clinics, outside tours, and the chance to just gather, meet, and talk, I'm really looking forward to September! As a reminder, if you are not registered, please do so. We would love to see you and there are certain fixed costs that registration does pay for... Yes, our Division will have a September meeting, and Mark Bridgewater will be discussing moving/relocating your model railroad. I hope to never have

to do this but, should I ever have to, I think this clinic will be of great interest.

I think I've taken enough time from work and the brass flat car I'm adding weathering to. I really hope you enjoy your Summer and I really look forward to seeing many of you in Cartersville in September.

Until next time, Engage!

—Perry Lamb
SUPERINTENDENT, PIEDMONT DIVISION
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PIEDMONT DIVISION

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HELP OUR DIVISION BY VOLUNTEERING!

Piedmont Division monthly meetings are held at:

Holy Innocents' Episcopal Church

805 Mt. Vernon Highway, Atlanta, GA 30327



Division meetings are open to everyone. Come join us to see what fun Model Railroading is. See Division Calendar for details.

We hope to see you at our next meeting!

BLUE OX TRAINS

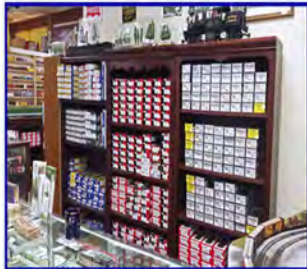
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DIRECTOR OF FINANCE REPORT

June Transactions:		Income	
	Admin Income		
	Meeting 'bucket' contributions		\$169.00
	Meeting 'paypal' contributions		\$22.37
	Raffles/Auctions		
	6 Pied...	D	\$20.00
	Total Admin Income		\$211.37
	SER 2023 Income		
	Miscellaneous		\$20.00
	Registration Fees		\$1,188.88
	Timetable ads		\$280.00
	Total SER 2023 Income		\$1,488.88
	Train Show Income		
	Miscellaneous		-\$68.00
	Train'n Camp Fees		\$13.99
	Total Income		\$1,646.24
	Expenses		
	Admin Expense		
	Meeting Room Rental		-\$300.00
	Software & Internet		-\$170.50
	Total Admin Expense		-\$470.50
	Train Show Expense		-\$115.68
	Train'n Camp Expense		
	Marketing		-\$161.85
	Total Expenses		-\$748.03
	Total		\$898.21

June Assets Increase \$898.21; note that SER Convention account, with a balance of nearly \$21,000, will end up close to break even after expenses are paid.

ACCOUNT	2023 1/31/23	2023 2/28/23	2023 3/31/23	2023 4/30/23	2023 5/31/23	2023 6/30/23
Assets						
~ Cash						
Fidelity	\$41,985.88	\$41,994.74	\$41,994.74	\$41,994.74	\$42,028.12	\$42,028.12
Piedmont Div Train Show	\$19,885.11	\$15,392.11	\$39,840.99	\$39,516.47	\$27,230.45	\$27,046.77
Piedmont Division 6586	\$3,233.19	\$2,809.13	\$3,223.40	\$2,922.68	\$13,383.41	\$12,976.42
SER 2023 Convention	\$3,950.34	\$5,066.20	\$7,786.82	\$14,800.68	\$19,275.65	\$20,764.53
Total Cash	\$69,054.52	\$65,262.18	\$92,845.95	\$99,234.57	\$101,917.63	\$102,815.84

—Jim Foley, Director of Finance
finance@piedmont-div.org
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PIEDMONT DIVISION PILOT OUTREACH PROGRAM (PD-POP)

On Sunday, July 9 the Piedmont Board held a working session to vote on and approve the following resolution:

Whereas, the Division supports the concept of a joint effort between the Division and SPONSORED AREA GROUPS (SAGs) hosting watch parties, instructional sessions and other activities spread across our Division territory.

Therefore, the Piedmont Division Board of Directors moves to establish SPONSORED AREA GROUPS.

TERM: Balance of 2023 and two full years after that. Would be re-evaluated at the end of 2024 & 2025 to validate that objectives are being met and the general response from the members in the groups.

OBJECTIVES:

- Increase on-line viewership by 20% by the end of the first year of the program
- Increase Division-wide training programs by 25% by the end of the first year of the program
- Increase membership by 10% by the end of the first year of the program

2023 BUDGET: Amendment for funding 2023 - \$500 line item added to the budget.

SAG STEERING COMMITTEE: Establish a steering committee to coordinate and guide the groups. Should consist of one BOD member as chairperson, and representatives of the SAGs participating in the program.

GUIDELINES: The BOD will approve operating guidelines and processes in the Piedmont SAGA Guidance document. This document may be modified by the SAG Steering Committee, as needed, and submitted to the Board of Directors for approval. The Board of Directors will review the SAG process annually at the May BOD meeting and the Division Planning meeting in December.

SUPPORTED AREA GROUP ACTIVITIES (SAGA) GUIDANCE

THANK-YOU for being the catalyst to enable the group of Piedmont Division members in your area to participate in Model Railroad activities.

This document provides guidance on how the Division will support your activities.

In particular:

- reach out to model railroaders to show them the benefit of joining the NMRA
- provide clinics or make and take, branded as Piedmont Division Train 'N Camp programs, at a location and time (Saturdays?) convenient to members outside of the Marietta/Acworth corridor
- promote watch parties for the Division meetings
- promote operating sessions for new operators.

The Piedmont Division members hosting a SAGA should:

- work with their network to find a suitable venue, for example a library meeting room, or Model Railroad hobby shop meeting room, members home, or some such which is at most half an hour's travel time for all members of the group
- copy the Division Directors of Administration and Operations ALL information sent to members about their activities. Failure to do so may lead to the Division removing their sponsorship.

SAGAs will be added as Events on the Division Website. Date, location, and other necessary details should be communicated with the SAG Steering Committee. That committee will post events on the Division Website.

A Sponsored Area Group will consist of four or more Piedmont Division members. The SAG Group will be completely informal and without officers. The Area Group members may be part of a 100% club, or an established operating group, or similar, but that is not a pre-requisite for sponsorship and any office held in the 100% club etc. will not apply to the Sponsored Area Group.

One Division member of the Sponsored Area Group will serve on the SAG Steering Committee reporting to the Division Board of Directors. This steering committee will also include a Division Board of Directors member.

(continued on next page)



NMRA PARTNERSHIP PROGRAM Like saving money? Your membership can save you up to 25% from more than 50 manufacturers and distributors of model railroad equipment, supplies and services.

Simply login to the NMRA.ORG website and highlight the "Benefits" tab on the member home page. Then click on "NMRA Partnership Program" in the pull down menu. This will take you to a page listing all of the participating vendors with detailed instructions on how to take advantage of their discounts. It even includes links to the vendor websites. Nothing could be easier!

Start saving today by taking advantage of this outstanding NMRA member benefit!

PIEDMONT DIVISION PILOT OUTREACH PROGRAM (PD-POP)

(continued from previous page)

Sponsorship may include one or more of the following:

- reimbursement of pre-approved expenses, for example room hire, clinic materials, to a maximum of the agreed cap or the limit to the number of reimbursements made to each Supported Area Group
- loan of Division audio visual equipment
- attendance by Division official or MMR.

Pre-approval of expenses to be reimbursed, or typically paid directly to the supplier, will require in addition to the amount[s]:

- where appropriate, quotes and timing of the expense
- an outline of the activity/group of related activities e.g., two-part clinic
- description of target audiences
- plans for promotion of the event
- and who (Piedmont Division member[s]) will conduct and manage the event.

This is to be submitted to the SAG Steering Committee at least 30 days before the activity (event).

If the expense exceeds our by-laws rules for the Treasurer (>\$100, not in budget), they will bring the request to a Board Meeting for approval.

It is expected that every SAG will pass the hat to cover some of the expenses of the event.

For advanced skill training, there will be a nominal fee (\$15) to cover costs and Rail Passes or Full NMRA Membership <https://www.nmra.org/trial-membership> will be required for all participants.

Attendance to a SAGA shall be open to all NMRA members and guests subject to NMRA rules (no more than 3 activities without either obtaining a Rail Pass or full membership of the NMRA).

—Piedmont Division Board of Directors

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PIEDMONT DIVISION CALENDAR

PIEDMONT DIVISION MEETING

TUESDAY, JULY 11 6 PM BOD; 7-9 PM Division Mtg
PIZZA AND SWAP MEETING Pizza supplied by Division
Bring whatever you want to sell!

PIEDMONT DIVISION LAYOUT OPEN HOUSE

SUNDAY, JULY 16 **JERRY MICHNEWICZ MMR** 1-5 PM
Jerry's New York Central Details on *Members Only Events* tab on our website at piedmont-div.org

PIEDMONT DIVISION MEETING

TUESDAY, AUGUST 8 6 PM BOD; 7-9 PM Division Mtg
CLINICIAN: **JIM DAVIS** TITLE: **CRICUT MACHINE USED FOR MODELING**
BRING & BRAG: Structures (especially paper ones) or whatever you have been working on and want to share.

63RD ATLANTA MODEL TRAIN AND RAILROADIANA SHOW

SATURDAY, AUGUST 26 9 AM to 5 PM
Golden Spike Show. Gas South District Convention Center, 6400 Sugarloaf Parkway, Duluth, GA 30093. For more info go to www.gserr.com

PIEDMONT DIVISION MEETING

TUESDAY, SEPTEMBER 12 6 PM BOD; 7-9 PM Division Mtg
CLINICIAN: **MARK BRIDGEWATER** TITLE: **MOVING/RELOCATING LAYOUTS**
BRING & BRAG: TBA

2023 SER CONVENTION—CARTERSVILLE EXPRESS

THURSDAY, SEPTEMBER 14 thru SUNDAY, SEPTEMBER 17
Clarence Brown Conference Center, Cartersville, GA
To register and for info go to 2023serconvention.org

GEORGIA LIVE STEAMERS

SATURDAY, OCTOBER 7 1 PM to 4:30 PM
Train rides. 1632 Centennial Olympic Parkway, Conyers, GA 30012. For more info go to northgeorgialivesteamers.org

PIEDMONT DIVISION MEETING

TUESDAY, OCTOBER 10 6 PM BOD; 7-9 PM Division Mtg
HONORED GUEST SPEAKER: **GORDY ROBINSON, NMRA PRESIDENT**
BRING & BRAG: TBA



ACHIEVEMENT PROGRAM NEWS



The following Piedmont Division members have recently earned AP certificates:

- Scott Dunlap - Association Volunteer
- Randall Watson - Civil

One of the requirements for the Master Railroad Engineer - Civil AP Certificate requires scratch-building three different pieces of trackwork. There are many types of acceptable trackwork listed on the NMRA's website. However, most frequently chosen are turnouts, single crossovers, crossings, and gauntlet tracks. Each of the three pieces of track must earn a Merit Award. In evaluating a piece of trackwork, judges use three criteria -- Scratch-building, Conformity, and Construction -- all



RANDALL WATSON
 RECEIVED MERIT AWARDS FOR TRACKWORK RESULTING IN HIS CERTIFICATE OF ACHIEVEMENT FOR MODEL RAILROAD ENGINEER—CIVIL

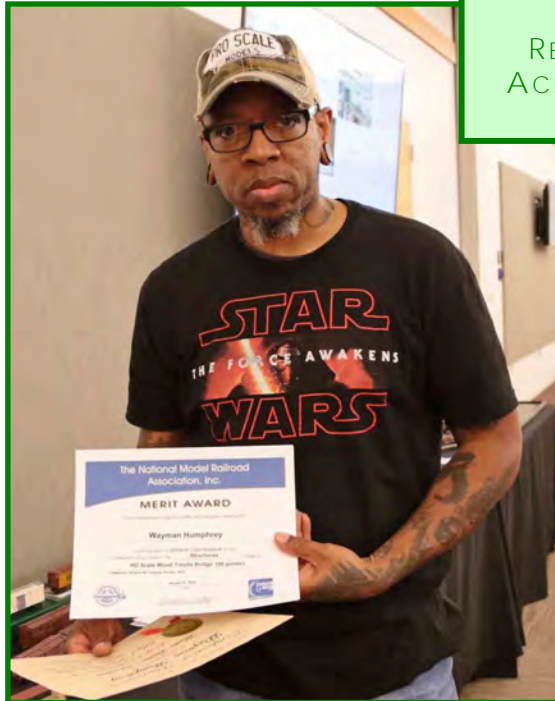
on a PASS/FAIL basis. For Conformity, an NMRA track gauge is used to insure NMRA standards are met. For Construction, one must successfully run a locomotive through all routes. Scratch-building is self-evident. Commercial frogs are not allowed.

The NMRA does allow the use of Fast Tracks jigs and fixtures in scratch-building trackwork. The Piedmont Division owns a num

ber of HO-scale FAST TRACKS jigs and fixtures and allows division members to borrow these materials for the express purpose of working on the Civil Certificate. Once a year the Division also conducts a "hands-on" clinic for the purpose of building an HO-scale turnout using the FAST TRACKS system. These clinics are limited to



SCOTT DUNLAP
 RECEIVED HIS CERTIFICATE OF ACHIEVEMENT FOR ASSOCIATION VOLUNTEER



WAYMAN HUMPHREY
 RECEIVED MERIT AWARDS FOR HIS BUILDINGS RESULTING IN HIS CERTIFICATE OF ACHIEVEMENT FOR MASTER BUILDER—STRUCTURES

six participants and all materials are furnished. Each participant is given a list of tools he or she must bring to the clinic. One should be reasonably adept at soldering before participating in this clinic.

—Charles Mason MMR
 Piedmont Division Achievement Program Chairman

ap@piedmont-div.org
 770 993-1589



CARTERSVILLE EXPRESS 2023 SOUTHEASTERN REGION CONVENTION

The PIEDMONT DIVISION would like to welcome NMRA SER members and guests to the CARTERSVILLE EXPRESS 2023 SER CONVENTION. The convention will be from Thursday Sept 14 thru Sunday Sept 17, 2023, at the Clarence Brown Conference Center in Cartersville, GA.

Go to 2023serconvention.org for complete details.

Register today!



SHOWCASE YOUR PIKE!

We are looking for Modelers who want to share their model railroads with other NMRA members from around the Region and the Nation who will be coming to the Cartersville Express SER Convention.

Specifically, we are looking for layouts to open on Sunday, September 17 as attendees return home.

But any other convention date will also work.

Layout visitors will only be convention attendees--not the public

Please contact Jim Datka at timetable@piedmont-div.org or call 770-772-0457



NEW 100% NMRA CLUB IN CARTERSVILLE, GA

BY SCOTT PERRY

We are proud to announce the formation of a new 100% NMRA Club in Cartersville, GA called the *GEORGIA NORTHWESTERN MODEL RAILROAD CLUB* (GNWMRC). This new club will serve NMRA members in the Cobb, Bartow, Paulding and Cherokee county area of Georgia. The club is founded by the North Atlanta Rail Barons (NARB), another 100% NMRA Club.

We have discovered that this area is a hot bed of model railroad activity, and we even have our own hobby shop, Fusion Scale Models. Cartersville is where we have our annual train show and also where this year's SER convention is being held, so it's very active for model railroading. Many of the Piedmont Division's new members are from this area as well.

After talking with many members of the NMRA we decided that we really need more training and education for the new modelers that are in our area.

Aside from the Model Railroading 101 program which was heavily attended last March, there really isn't anything in the way of education on the basics of model railroading. New people regularly contact NARB for help with their model railroads so we decided it was time we do something about it.

The GNWMRC will have monthly meetings at the *CENTER COMMUNITY CENTER* in Cartersville, GA. The newly refurbished meeting facility has room for 40 attendees with chairs and tables and even includes a kitchen we can use. It is just 2 miles off Exit 290 on I-75 and conveniently close to the Cracker Barrel in Cartersville. This central location makes it easy for anyone in the four-county area to get to the meeting in about 30 minutes or less.

We will be featuring clinics more focused on new model railroaders and those that are just coming back to the hobby, but our clinics will have tips and tricks for intermediate and advanced modelers as well. There will also be special events on weekends that are hands on, simi-

lar to Piedmont's *TRAIN 'N CAMPS*, but more focused on model railroading basics such as benchwork assembly or model building. The GNWMRC will also have a focus on field trips and other social events using the fancy kitchen!



Scott Perry is part of the National NMRA *MEMBERSHIP RETENTION PROJECT*, so this new club is also an experiment on recruiting and retaining members by delivering benefits to these new members such as friendship/camaraderie, education/training and one-on-one mentorship. Our goal is to add another 75 members to the NMRA in less than two years, and retain them for a minimum of two years. The results of the program will be reported month to the Retention Program Team and our President Gordon Robinson.

Another feature of the new GNWMRC is that many in this four county area have a problem driving to our Piedmont Division Meetings. Sometimes, just from

Cartersville, it can take over an hour one way. Others have work or other outside commitments on the second Tuesday of the month and just can't make it that night. This will provide an alternate venue on the Third Thursday of the month to meet with other NMRA members and learn something new and fun. We are also discussing having a Watch Party for the Piedmont meeting if we can get the technology and funds together to do it.

The last important feature of the GNWMRC is training volunteers to help the Piedmont Division. NARB is always a heavy supporter of the *PIEDMONT DIVISION MODEL TRAIN SHOW* and other activities for the Division. One of the best ways to "plug in" a new member is to get them volunteering. At the last train show we did this very thing and got many of our new members to help. Now we can recruit and train more of them to participate in Piedmont activities.

(continued on next page)

NEW 100% NMRA CLUB IN CARTERSVILLE

(CONTINUED)

Our clinic program for the first three months is as follows:

May 18 – Basics of Benchwork by Scott Perry. Learn about open grid, L-girder, around-the-wall and other types of benchwork.



June 15 – Why use DCC and How to Choose a System by Howard Goodwin. Learn about DC vs DCC, how to select a DCC system and basics on how DCC works, decoders and much more!

July 20 – Track Planning Basics by Scott Perry. Learn how to plan your first model railroad including space constraints, research, the critical questions, design tools, and formulating your first track plan.

July 29 – Railfan Event – The Georgia Northeastern. Visit the Tate Yard and Elizabeth Interchange in Marietta and see antique locomotives that run during the week. Also see key industries, and the Turniptown curved trestle.

Please come and join us for a meeting and have some fun. Be sure to bring your NMRA Membership Card for admission and there is no charge for the meeting, but donations are gladly accepted. Your NMRA dues must be current to attend as per NMRA 100% Club rules!

Our meetings are on the third Thursday of every month from 6:30 pm to 8:00 pm. We meet in Cartersville at the **CENTER COMMUNITY CENTER** at 682 Center Road, Cartersville, GA 30121. The meeting room is 2 miles from the Canton Hwy 20 exit off I-75, the third Cartersville exit #290. Get off the exit and go east to the red light at the Wendy's restaurant and take a

right. Go almost 2 miles and take a hard right at the Bait Shop, then your first left to the Community Center.

For more information contact: Scott Perry, scottgperry01@gmail.com, 803-504-9392

—Scott Perry



NORTH GEORGIA MODURAIL Invites the Public to Enjoy a Multi-Club MODEL TRAIN EXHIBIT at SMOKE RISE BAPTIST CHURCH

5901 Hugh Howell Road, Stone Mountain, Ga.

Saturday, July 29 9 am to 5 pm
Sunday, July 30 Noon to 3 pm



For More Information, email us at
mailcar@northgeorgiamodurail.org

... or Check our Web-Site!
www.northgeorgiamodurail.org



PIEDMONT DIVISION'S APRIL CLINIC

WALT LILES: WEATHERING DIESEL LOCOMOTIVES



[Holy Innocents informed us that they would be closed just two weeks before our scheduled meeting date. Fortunately our interwebs experience let this presentation go online without a hitch.]

Walt showed us how to create our own reality. At least on our locomotives.

He walked us through the weathering products and tools he uses most, how he uses them and the effects they create. He showed how he used these to weather three diesel locomotives.

Walt's key message in creating realistic weathering: study how the prototype was used, abused and maintained in the era you are modeling.

There are many, many photo resources available in books and online to show you what locomotives look like in the real world. It is easy to find specific engines from nearly every railroad at many different times during their length of service. You are also likely to find the specific engine you are modeling.

PRODUCTS FOR WEATHERING

- **AK INTERACTIVE PAINTS & PENCILS**
- **VALLEJO PAINT PRODUCTS**
- **TAMIYA-PANEL LINE ACCENT COLOR**
- **PAN PASTEL –WEATHERING PASTELS**
- **MODELERS DECALS AND PAINT-ACRYLICS**
- **ALPHA ABRASIVES –MICROBRUSH APPLICATORS**
- **PAINT BRUSHES- PURCHASE VARIETY PACK AT ART STORES ON SALE WITH A VARIETY -**

Weathering Diesels

- **What effect are you looking for ?**
- **What region are your locomotives running?**
- **How aged do you want to weather ?**
- **Suggest looking at prototype .**
- **Study older photos or find books on your favorite railroad to see dates and conditions of locomotives.**
- **Start simple, lite and add little bits at a time.**
- **Different weathering techniques show up different in lighting**

The goal is to weather your locomotives to be an accurate representation of the prototype based on the goods it hauled, the environment it ran in and the level of maintenance it received during your specific era.

Walt recommended starting with one weathering product at a time, apply it with light touches and slowly build to the finish you want. It is easy to overdo so check your work often.

He pointed out that colors will look different under the various types, colors and intensities of lighting—check your weathering on the layout where it will run.

Weathering products were discussed at length, and he went into detail about the AK pencils. These pencils are used by military modelers to highlight seams and the smallest details with little skill or effort and no clean-up.

continued on next page



PIEDMONT DIVISION'S APRIL CLINIC

WALT LILES: WEATHERING DIESEL LOCOMOTIVES



Walt took us step-by-step how he weathered three different locomotives. He chose a wide range of livery colors and represented distinct levels of weathering based on time in service, what they hauled and how they were maintained.

ATLAS GP-38 FRISCO

FRISCO Locomotives ran a pool power during the 1970 and 1980's with SCL. This is a good loco paint scheme to try things on for weathering today. This is a fresh new engine.



The first is a bright Atlas GP38 in bold Frisco orange and white, which better shows the effects he is applying.

He applied only light weathering to this locomotive.

One detail he added was spillage of diesel fuel on the fuel tank, something which was common in the 1970s.

The intakes were highlighted with a wash, and the top received fading, and additional grime and soot from the exhaust.

He showed how simple accents can turn trucks from boring black blobs to eye catching details.



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PIEDMONT DIVISION'S APRIL CLINIC

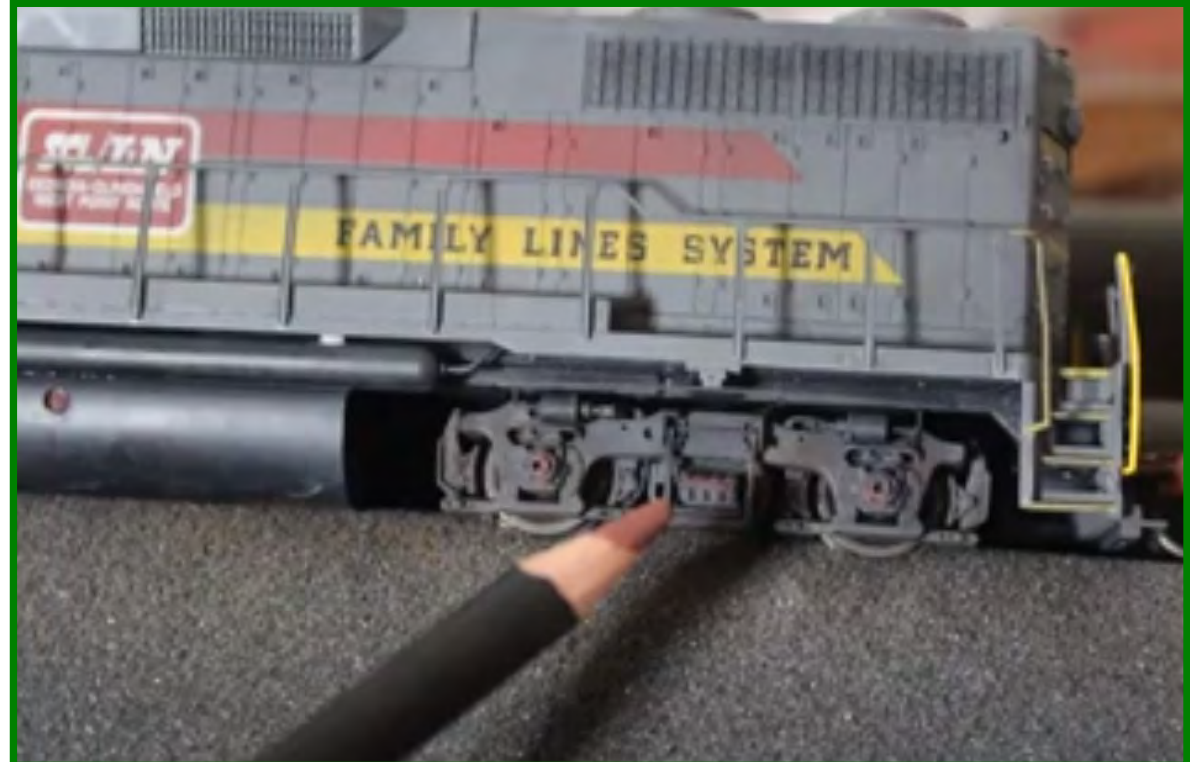
WALT LILES: WEATHERING DIESEL LOCOMOTIVES



The second locomotive is a dark gray Atlas GP38 in Family Lines System (Louisville & Nashville, Clinchfield, Seaboard Coast Line) that was used in heavy coal hauling.

For this model, Walt used PanPastels rather than washes on the intake grills and roof.

He showed how gray and black powders look different on a dark locomotive.



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PIEDMONT DIVISION'S APRIL CLINIC

WALT LILES: WEATHERING DIESEL LOCOMOTIVES



The third is a brightly colored Chessie Atlas GP-7. This locomotive was also in heavy coal service and it received the heaviest weathering.



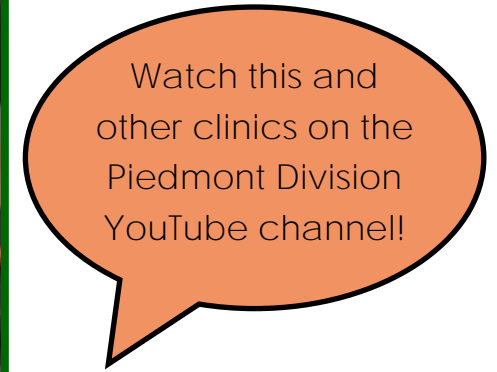
Walt applied this in layers to show how he built it up. This provided examples of where you may want to stop on your own model. rather than the heavy weathering of this model.

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PIEDMONT DIVISION'S APRIL CLINIC

WALT LILES: WEATHERING DIESEL LOCOMOTIVES



With these three models, Walt showed techniques specific to every part of the locomotive: foot boards and walkways, railings and grab irons, intakes and exhausts, access panels, fuel tanks, roof, trucks and couplers.

He showed how to represent the impacts wear, grime, soot, heat, sun and rain have on each part, and how these accumulate over time. Walt explained how the commodities hauled and maintenance practices can be reflected in your weathering as well.

It's all about knowing what the prototype locomotive does and how it is operated and maintained that should guide your weathering projects. Walt showed us how to apply this knowledge to produce the most realistic models possible.

Walt closed with a teaser: he has received the highly detailed ScaleTrains SD45 in Southern Pacific livery which ran all over CSX in the 1990s. SP is notorious for running super dirty locos.

He says stay tuned.

—Jim Datka



PIEDMONT DIVISION'S MAY CLINIC

STEFAN BARLETSKI: *TATE DEPOT MUSEUM AND MODEL RAILROADERS*



Photos by James Bando

Stefan reported on the ongoing efforts to renovate the Tate Depot, and presented the layouts being supported by the Tate Depot Model Railroaders.

Tate, Georgia was established as company town for the Georgia Marble Company. The depot was built in 1916 and served the community until 1949 when the L&N Railroad discontinued passenger service. The depot features a large freight room which served the marble quarry.

In 2011, an N scale train layout was added showcasing Jasper, Tate, Nelson and the Marble Valley branch line including its switchbacks. There were questions as to who owned the depot and this layout was later dismantled and stored in an administration building in Jasper.

The depot was slated for demolition as it blocked the ability of cars and trucks from seeing trains on the rail line. In 2015 the depot was relocated nearby and restored by the Marble Valley Friends group to serve as a community center. The depot was intended to be an event and meeting venue that compliments the businesses in the Tate Commercial District.

In 2021, Bill Gibson learned of the empty depot and decided to donate his professionally built HO model train layout. His Vinings Northern Railroad is based on the Etowah Old line and is set in the late 1950s to early 1960s. It runs from Vinings through the mountains to the Georgia Marble Company's Tate quarry.

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PIEDMONT DIVISION'S MAY CLINIC

STEFAN BARLETSKI: *TATE DEPOT MUSEUM AND MODEL RAILROADERS*



Gibson's donation initiated plans to further develop the depot. The Tate Depot Train Society was formed as a non-profit to conduct fund raising and manage the facility.

A call went out for knowledgeable model railroaders for the installation of Gibson's layout as well as perform its day-to-day operations and maintenance. The Tate Depot Model Railroaders was formed under a charter of the Society.

Luckily the room Gibson's layout was in was about the same size and the old freight room. As his layout was built in Chicago and shipped to his Big Canoe home, breaking it down and transporting it to the depot was relatively easy. The new location was about 6 feet wider which allowed an additional section which includes a model of the Tate depot as well as a staging area.



Photo by Stefan Bartelski

The first open house with the newly installed layout was held October 15, 2022.

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PIEDMONT DIVISION'S MAY CLINIC

STEFAN BARLETSKI: *TATE DEPOT MUSEUM AND MODEL RAILROADERS*



VININGS



BLUE RIDGE



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Photos by Stefan Bartelski

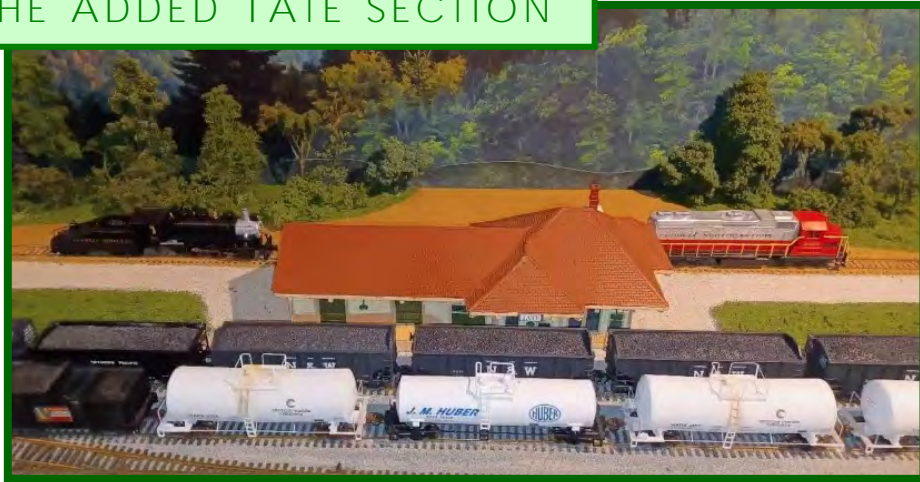


PIEDMONT DIVISION'S MAY CLINIC

STEFAN BARLETSKI: *TATE DEPOT MUSEUM AND MODEL RAILROADERS*



THE ADDED TATE SECTION



N SCALE LAYOUT



The long-lost N Scale layout was rediscovered and is now setup in the depot's waiting room.

Based on furniture found in the depot, a museum corner is being added as a dispatcher's office. Antique equipment, schedules, books and other items are being sought to outfit this appropriate for the early 20th century. Plans are being made for a "working" telegraph and to refurbish the semaphore handles still attached to the depot's ceiling.

The Tate Depot Train Society and Tate Depot Model Railroaders are restoring an important part of our local railroad history. They have displays and layouts that are a worthwhile destination for the public and are an excellent ambassador for our hobby.

Please support their efforts through contributions of your time, equipment, memorabilia and money.

For more info go to TateDepotTrainSociety.org and TateDepotModelRailroaders.org.



—Jim Datka

Photos by Stefan Bartelski



PIEDMONT DIVISION'S MAY CLINIC

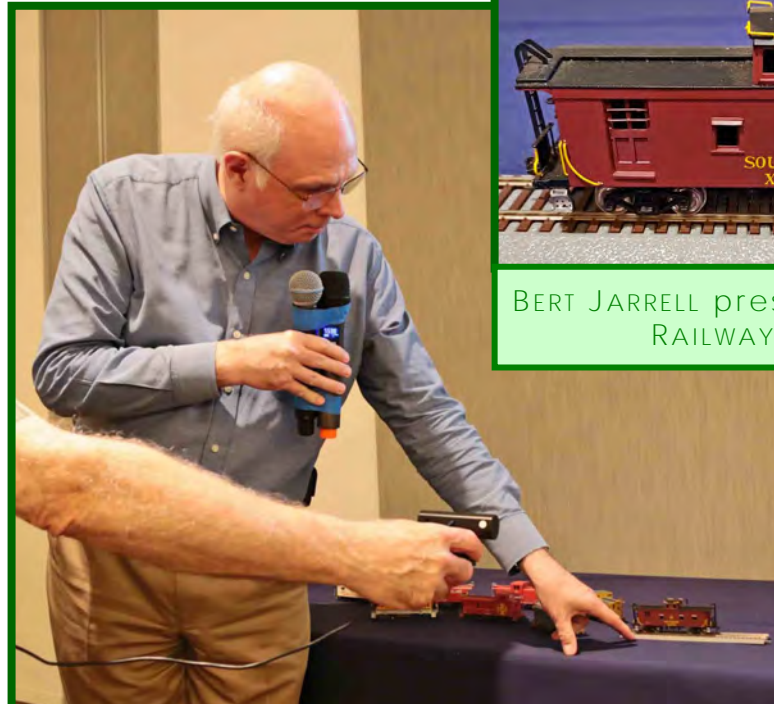
BRING-AND-BRAG: CABEASE!



MIKE DEATON brought in some of the cabooses and waycars on his COLORADO JOINT LINE RAILWAY SYSTEM



Photos by James Bando and Jim Datka



BERT JARRELL presented a SOUTHERN RAILWAY caboose



PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



Harvey scratch built his first boxcar at the age of 14 which he financed by cutting neighbor's lawns. He also built several *Silver Streak* kits which he believes are great for developing scratch building skills. These can be found today on Ebay.

Pressed Steel Car Corporation built 1000 of the Seaboard Class B3 boxcars starting in 1920. The last 4 were retired in 1965, so these cars are appropriate for any layouts modeling the late steam to early diesel eras.

The insides of these cars were 40' 6" long and 8' 6" high with a 6' wide door. They road on Andrews trucks.



NEW MEMBER
RICHARD STEWART

These cars have never been offered as ready-to-run models—which is why scratch building is your only option.

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PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



Harvey explained there are two critical things to know about scratchbuilding: it takes time and it is not cheap. He pointed out that the most highly detailed commercial models will cost less than a single one you make yourself.

But the big payoff is the pride and satisfaction you get from building rolling stock from scratch. That may be worth far more than your time or money.

Harvey pointed out there is an economy of scale because many of the parts and materials you purchase come in quantiles for more than a single car. Building several cars brings down the cost per car.

Let's get started.

Scratchbuilding a car is not a way to get cheap cars.

The payoff is in the satisfaction of completing a good project.

ITEM	MAKER & CAT. NO.	QTY	PRICE (See note 1 below)
Basswood, 3/16 in. x 4 in. x 24 in.	Northeastern 316STR4	1 EA	\$ 5.62 Floor & end blocks
	OR		
Basswood 3/16 in. x 4 in. x 24 in.	Hobby Lobby	1 EA	\$ 4.81 Floor & end blocks
Siding 1/32 in thick 1/16 scribed	Northeastern 116SCR132P	1 PG	\$ 3.80 Car sides
Basswood , 1/32 in. x 2 in. x 24 in.	Northeastern 132STR2	1 EA	\$ 2.02 Over roof
Roof Walk, Centerline	Tichy Train Group 3029	1 EA	\$ 2.50
Styrene strip .010 x .060, 10/PG	Evergreen Scale Mdls 103	1 PG	\$ 4.10 Roof ribs & end trusses
Styrene strip .010 x .080, 10/PG	Evergreen Scale Mdls 104	1 PG	\$ 4.10 Diagonal trusses
Styrene strip .010 x .060, 10/PG	Evergreen Scale Mdls 136	1 PG	\$ 4.10 Bottom edge of sides
Styrene strip half round 060, 5/PG	Evergreen Scale Mdls 241	1 PG	\$ 4.10 Diagonal trusses
Styrene strip, angle, .060, 4/PG	Evergreen Scale Mdls 291	1 PG	\$ 4.10 Overhang at roofline
Wire, florist's	Any craft or flower store	1 LT	\$ 3.50 Brake line
Wire, Brass, 028, 10/PG	Precision Scale Co. 4872	1 PG	\$ 6.00 Brake staff, cut levers & rods
Stirrup Steps Style A	A-Line 29000	1 PG	\$ 3.50
Bolsters, Riveted Steel, 6/PG	Tichy Train Group 3069	1 PG	\$ 3.10
Grab Irons, 18" drop type 100/PG	Tichy Train Group 3015	1 PG	\$ 3.50
Center Sill, Fishbelly	Tichy Train Group 3089	1 EA	\$ 5.00
Roof End, V-notched 7/8	Tichy Train Group 3058	1 PG	\$ 3.70

Many parts come in packages of several. Consider economy of scale and make more than one car.



A key learning from Harvey is to purchase all the parts and materials you will need up front. This can avoid future frustration—it may take time to find the specific parts you need.

He then said to plan your build. What are the steps and sequence to your assembly? This is where experience with commercial kits can build your levels of skill and success.

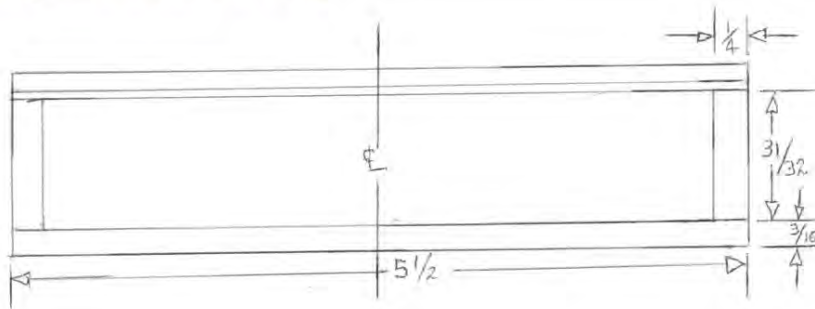


PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



Building the Basic Body



Glue floor, end block and roof together.

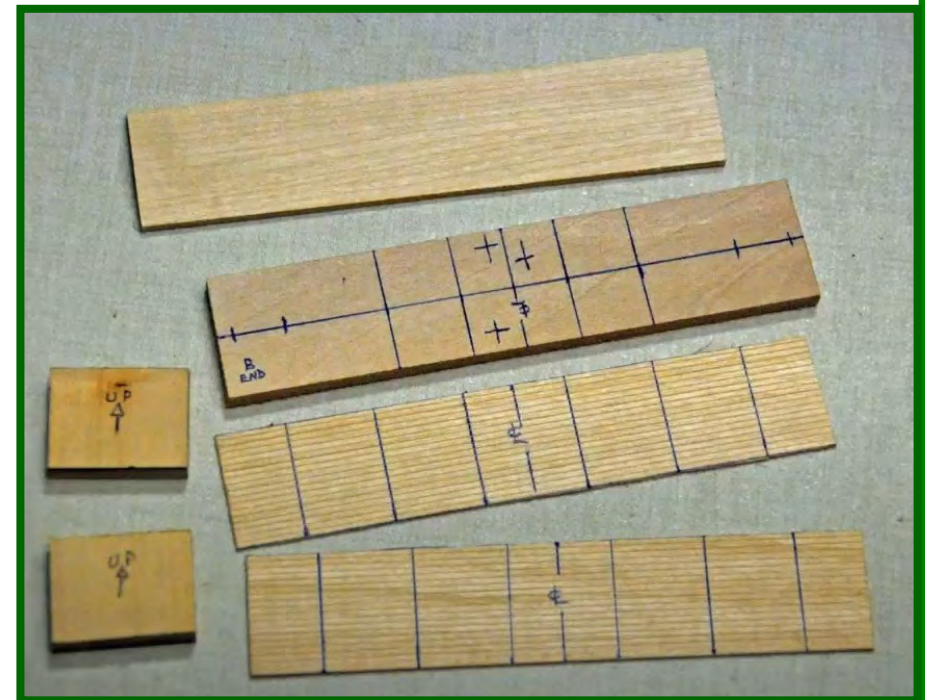
Use Elmer's Wood Glue for a strong permanent bond.

EVERYTHING MUST BE SQUARE.

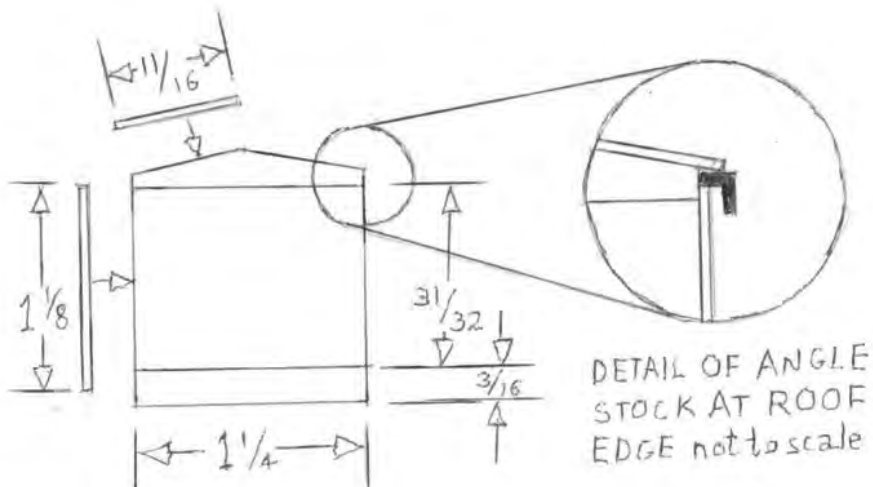
No Pablo Picasso train here.

Harvey starts with the box by cutting the wood or styrene to the finished sizes. Be certain to mark where key components will be mounted and drill pilot holes while they are still flat.

Building the box is the most crucial step. You need everything to be completely square and have good bond between the parts. This is essential to having a car that runs well. He recommends adding extra material or weights to the interior of the box as you glue it together.



Harvey illustrated several key details that need to be included in your build.



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PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



Harvey cautions when working with water based glues and finishes on wood as it will encourage warping. Be sure wood pieces are firmly clamped to each other and mounted on a solid base. Leave the piece in the clamps until you know the glue has completely dried and set.

Cutting and forming the braces, ribs and other details can be time consuming. Tools such as the NWSL Chopper can simplify this task. Templates are available for locating grab bars.

Making Parts

Tedious and repetitive – get parts made now for use later.



- 26 Roof Ribs
- 16 Side braces (ribs)
- 2 Coupler release levers
- 2 Air line retaining clips
- 2 Tackboards
- 2 Metal panels for doors

WARNING: Plastic solvent cement must be used with adequate ventilation.

Sides

1/32 in. thick siding or roof halves will **WARP** from the moisture in the glue.



Wood block used to keep side flat while glue sets.

Do this for both sides and roof halves.

Let glue dry and set before attaching another piece.

Harvey provided important tips for the sequence and installation of the many detail parts.



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PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



Ends

Drill for three grab irons.

Make 2 indentations and drill for eye bolts for coupler release bars.

Glue Murphy end to body.

Finish drilling holes into the body.

Cut ladder to 6 rungs and install.

Install tackboard and grab irons.

Slide eyebolts onto coupler release bars and insert into holes (Very tedious). This will take multiple attempts. Set with thin CA cement.

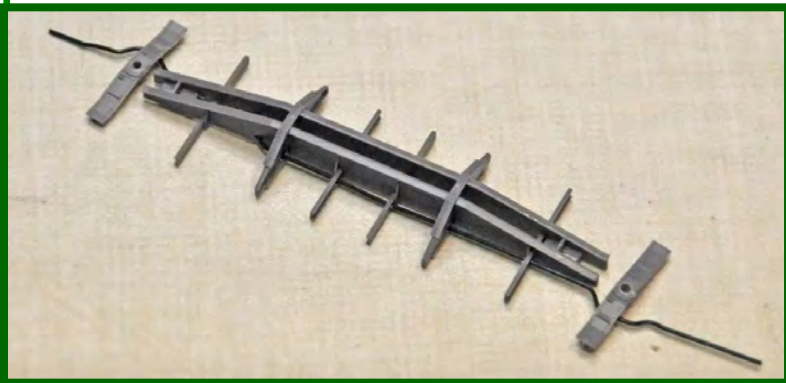
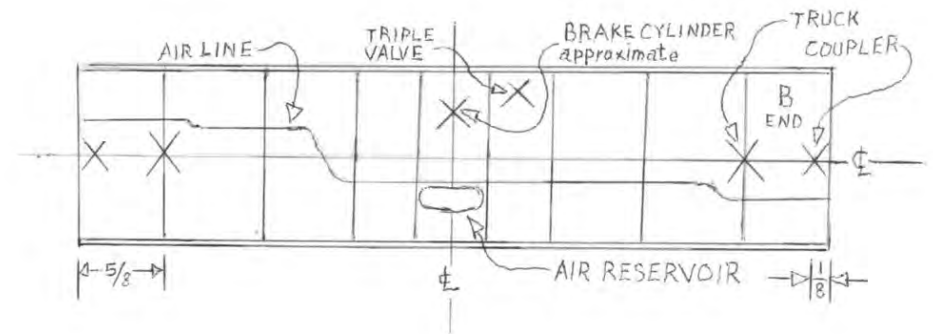
B end - Install brake ratchet, brake platform and brake wheel.

B end - Install brake rod. (Brake rod goes over the coupler release bar.)

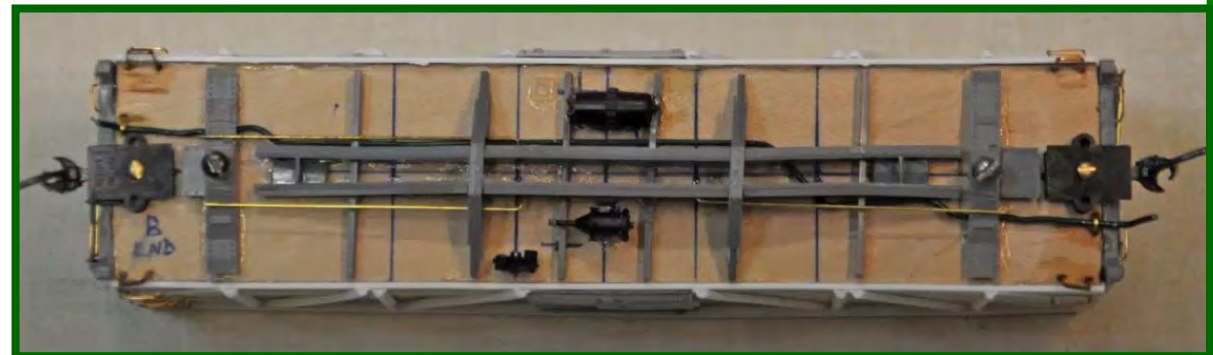


Harvey noted that the ends can be challenging but offered several tips to make this easier. He went into detail about the most critical steps and where you can find key parts. Harvey referenced his curse-word scale used when building out the car ends.

Underside



For the underside, Harvey recommends using Tichy Train Group's 40' Fishbelly Underframe to save time and is correct for this model. He pointed out several steps that are not included in the instructions for assembly and mounting the brake details.





PIEDMONT DIVISION'S JUNE CLINIC

HARVEY GEORGE: SCRATCHBUILDING A SEABOARD CLASS B3 BOXCAR



After mounting the trucks and adding the roof detail, your assembly should be complete. At this point Harvey says to check the coupler height and run the car on your layout. Any problems with running the car should be addressed before painting and decaling.

Harvey listed the coloring and lettering for this model. He specifically recommends using National Steel Car D-103 decal set as this set has the logo as Seaboard Railway which is accurate for the Class B3. Tichy decal set 10184 is appropriate for the B5 version and the logo refers to the later Seaboard Railroad.

He then covered levels of weathering based on the length of service in the era you are modeling.

Harvey explained how this model can be used for other rail roads as well. He also explained that truss rod cars could noticeably droop or crown in the middle if the rods are under- or over-tightened. He showed examples of how this can be modeled.

Assembly Complete

Drill grab iron holes and install the roofwalk.



Install Andrews trucks. Road test the car. Work out any issues.



Harvey showed that scratch building a box car can be done in steps that most modelers can follow. Building a one-of-a-kind piece of rolling stock would be the pride of any modeler.

He has produced a complete 18 page set of instructions for building this car which is available in pdf or MS Word format. Contact him for a copy.

Harvey will be repeating this presentation at the Cartersville Express SER Convention in September.

—Jim Datka



PIEDMONT DIVISION'S JUNE MEETING

BRING-AND-BRAG: SCRATCH BUILT OR KITBASHED ROLLING STOCK



NORM LUNDEN showed two garden scale cars from **Father's Trains** that he has beautifully detailed



MIKE DEATON brought his Atlanta and West Point boxcar and explained how the prototype found its way to Colorado



Photos by James Bando and Jim Datka



FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



Justin models the Pennsylvania Division of the Delaware and Hudson Railway from Carbondale to Wilkes-Barre, PA during 1977-79.

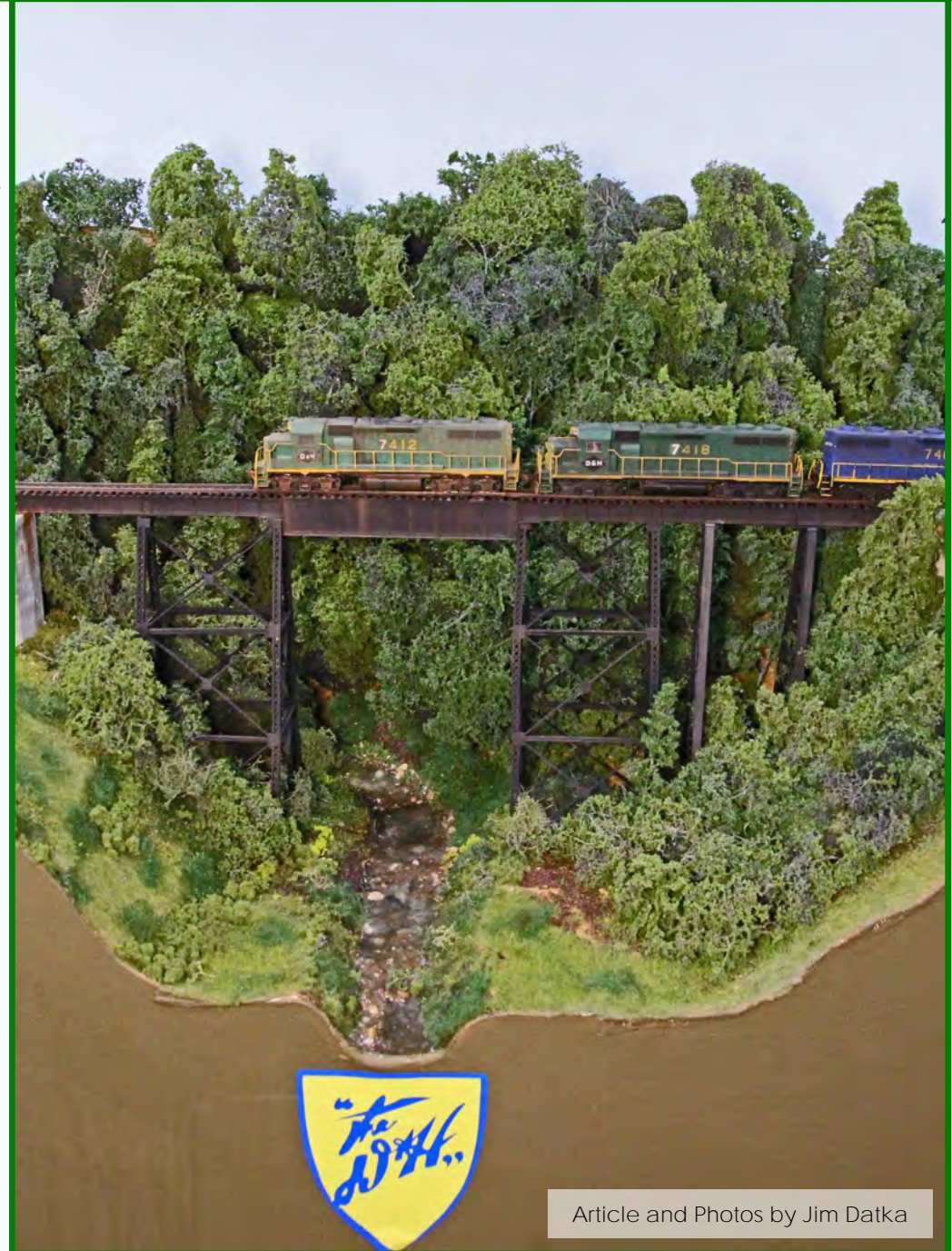


The forested rolling hills and varied industries of the area suits Justin's abilities to create realistic scenery and structures.

The financial troubles of the D&H at that time let him demonstrate his weathering skills to model a hard working railroad that may be scrimping on maintenance. The same skills are applied to the industries declining to become part of the rust belt.

JUSTIN WITH HIS JUNIOR ENGINEER
EASTON

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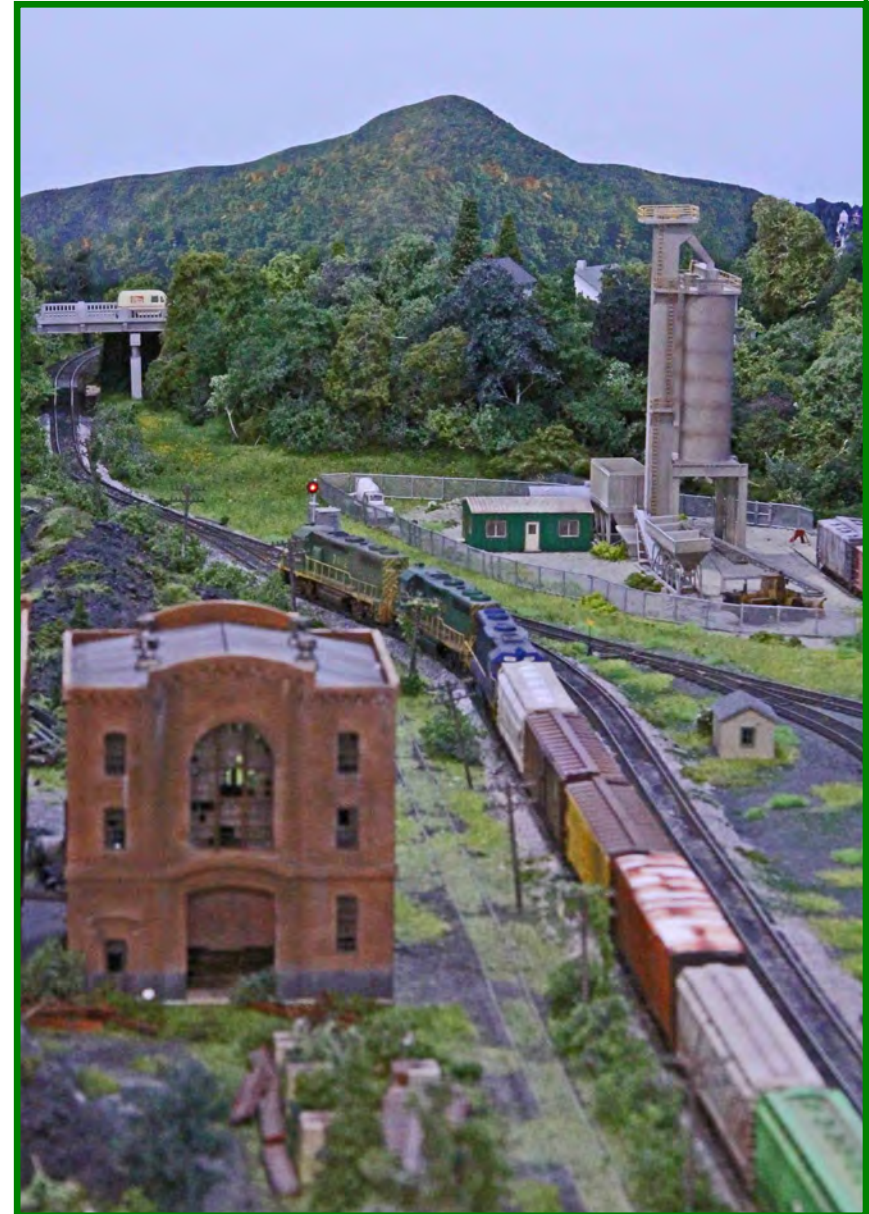
Article and Photos by Jim Datka



FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



The foreground plantings blend with the photo backdrops giving the illusion of a larger layout. It also provides a great representation of the Pennsylvania country side.



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FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



Selected industries from each town along the division are modeled. These cover many rail customers including brewers, cement, machinery, printing and agricultural products. Many are simply flats along the backdrop but still feature car spotting for operations. The diverse industries require a broad selection of car types for the many commodities and products shipped.



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FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



The standout feature of the layout is the trestle crossing the Starrucca Creek in northeastern Pennsylvania. Justin's compressed model is presented directly below a painting of the prototype by Robert Frascella. The scene is highly detailed and realistic, and the weathering of the bridge is impeccable.



(continued on next page)



FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



For a medium sized layout, it is well thought out. There are plenty of switching opportunities in the towns requiring a wide range of rolling stock. Towns are separated by short, scenic mainline runs to add distance between each location and helps to reinforce the Pennsylvania location.

A lower deck staging area adds to the operational capabilities and offers a access to a large number of possible engine configurations, car types and trains.

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FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



There are two things I really admire about Justin. First, his weathering abilities. No locomotive or car is put into service on his railway until it looks appropriate to its use and length of service. The engines show wear and renumbering appropriate for their age. Covered gondolas clearly show what they have carried. Box cars and buildings show the effects of use and abuse. He shared some prototype photos that guided his craftsmanship.

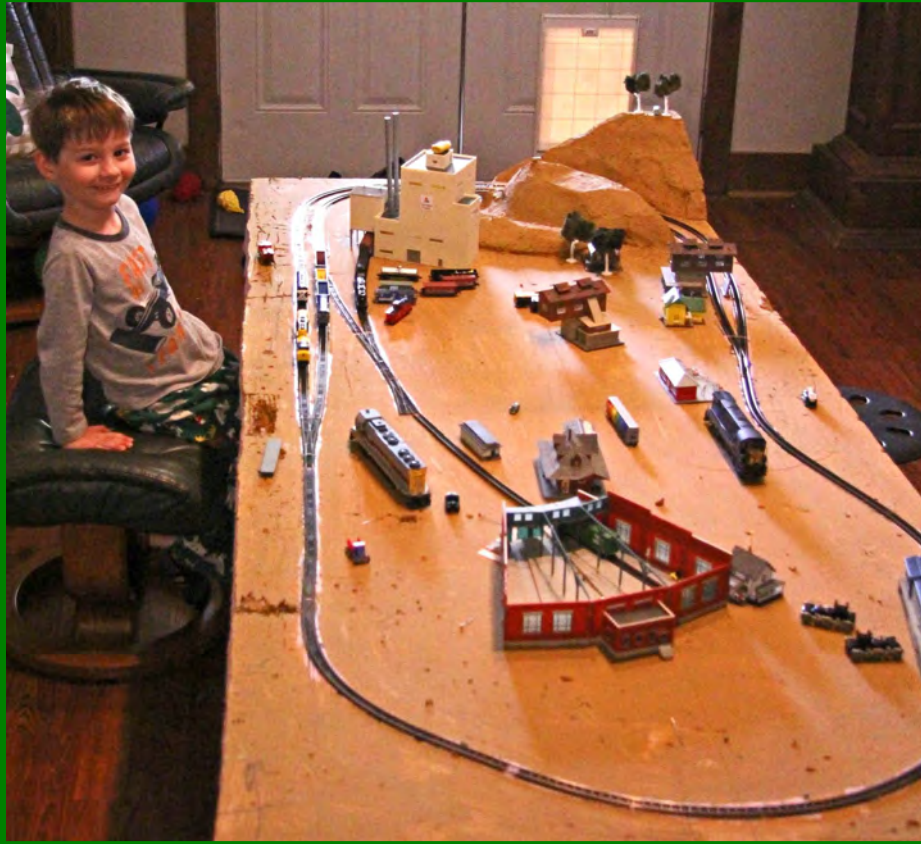


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FEATURED MEMBER LAYOUT: JUSTIN JANKAUSKAS'S *DELAWARE AND HUDSON RAILWAY*



The second thing I admire is his relationship with his son, Easton. Justin clearly encourages his interest in model railroading, from the unbelievable Brio trains that have taken over their family room to a newly started N scale layout of his own.

Easton LOVES trains! He helped explain the different locations on dad's layout and was a real pleasure during my visit.

Justin has fostered a future modeler with a real passion for trains.

—Jim Datka

